

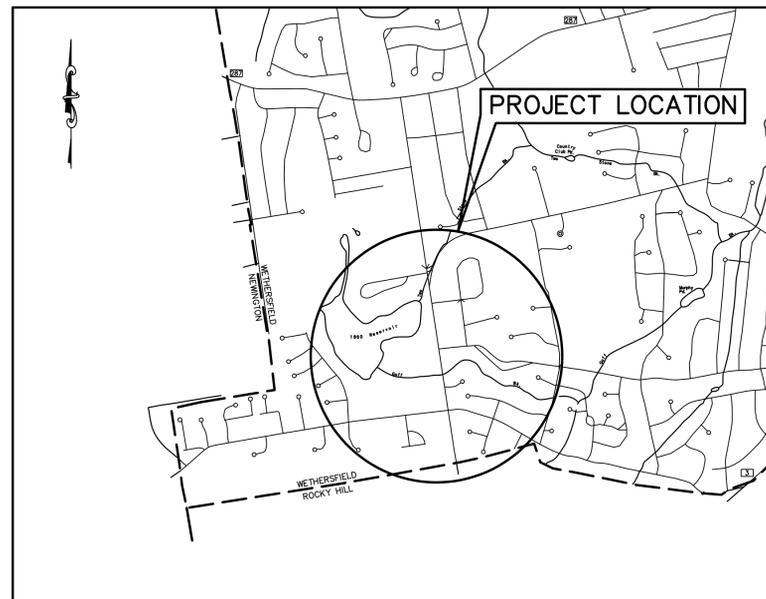
TOWN OF WETHERSFIELD DEPARTMENT OF PUBLIC WORKS ENGINEERING DIVISION



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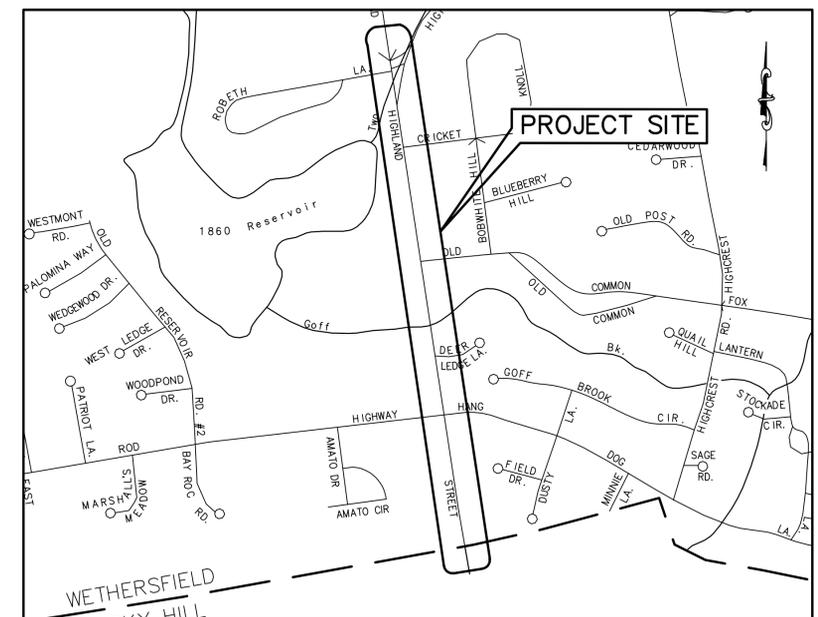
HIGHLAND STREET PAVEMENT REHABILITATION ROCKY HILL TOWN LINE TO THORNBUSH ROAD STATE PROJECT No. L159-0001

MAY 2020

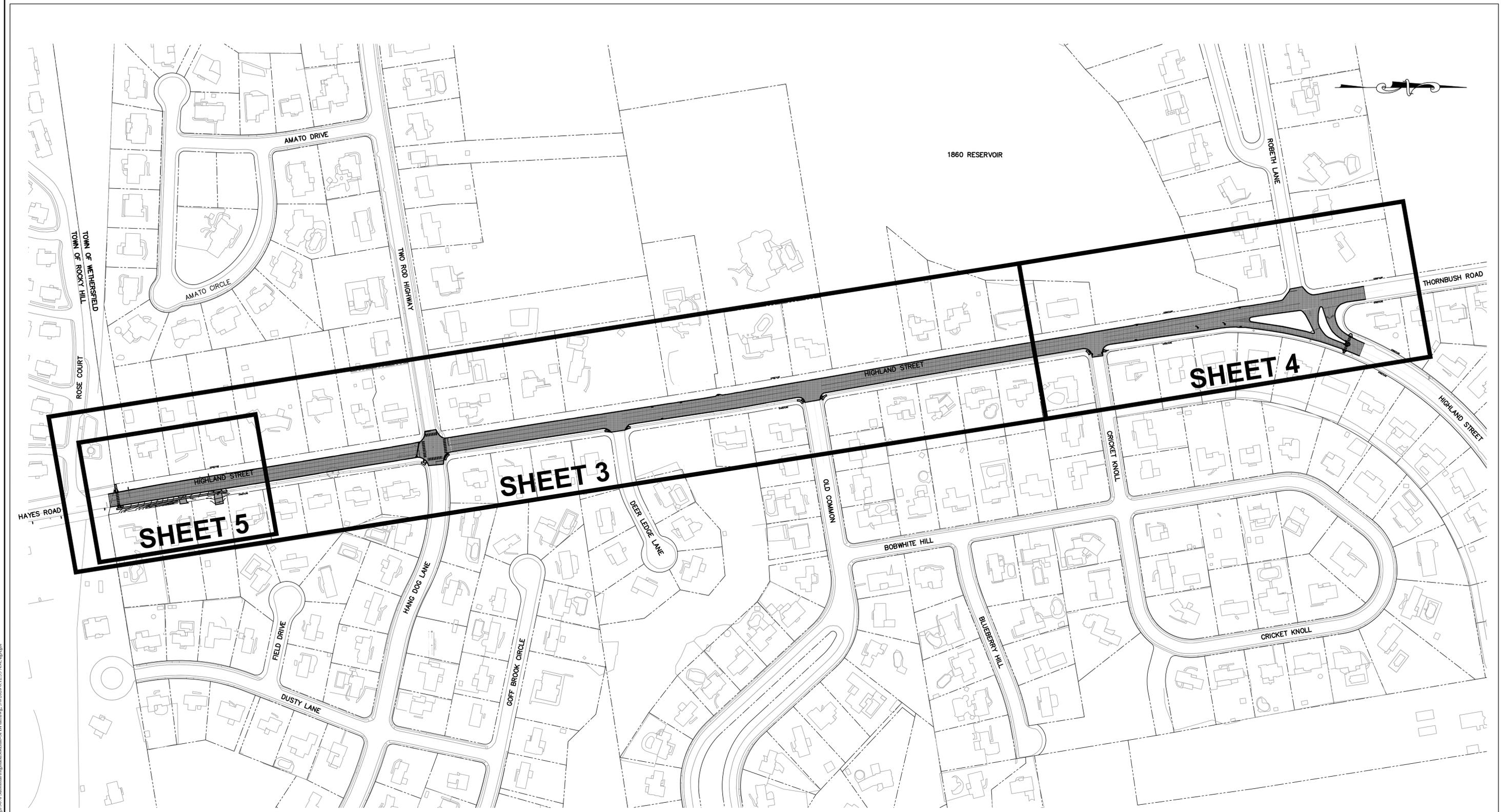


LOCATION PLAN
1"=1500'

LIST OF DRAWINGS	
SHEET No.	SHEET TITLE
1	COVER SHEET
2	INDEX PLAN
3	SITE PLAN
4	SITE PLAN
5	SIDEWALK EXTENSION PLAN
6	CONSTRUCTION DETAILS
7	CONSTRUCTION DETAILS



SITE MAP
1"=600'



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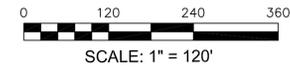
SHEET 5

SHEET 3

SHEET 4



TOWN OF WETHERSFIELD
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
 505 SILAS DEANE HIGHWAY
 WETHERSFIELD, CT 06109



HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
 STATE PROJECT No. L159-0001

INDEX PLAN

REVISIONS	
No.	DATE

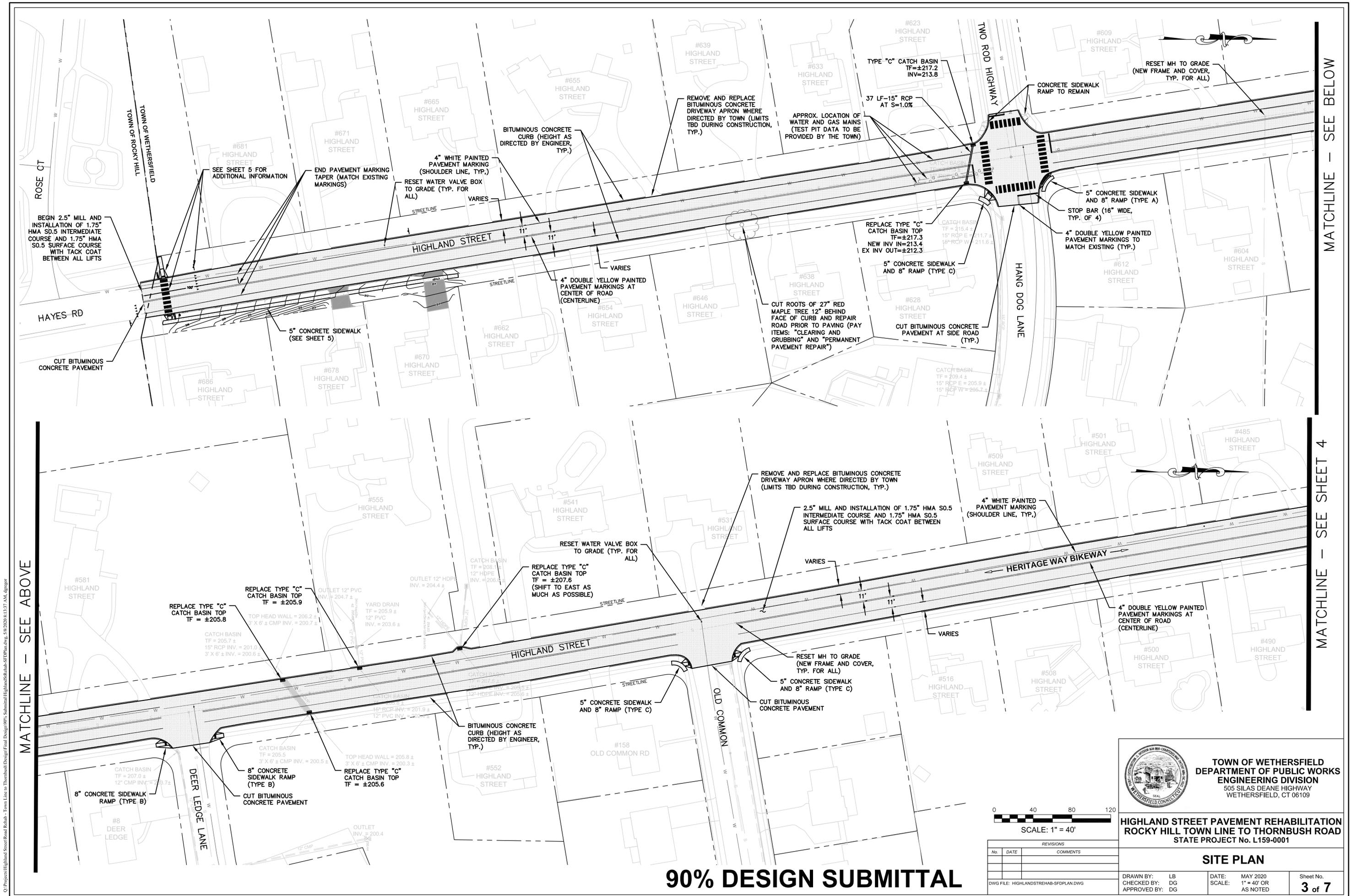
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DATE: MAY 2020
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Sheet No.
2 of 7

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MATCHLINE - SEE ABOVE

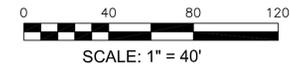
MATCHLINE - SEE SHEET 4

MATCHLINE - SEE BELOW

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HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
 STATE PROJECT No. L159-0001

SITE PLAN

REVISIONS		
No.	DATE	COMMENTS

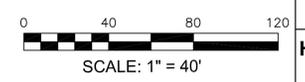
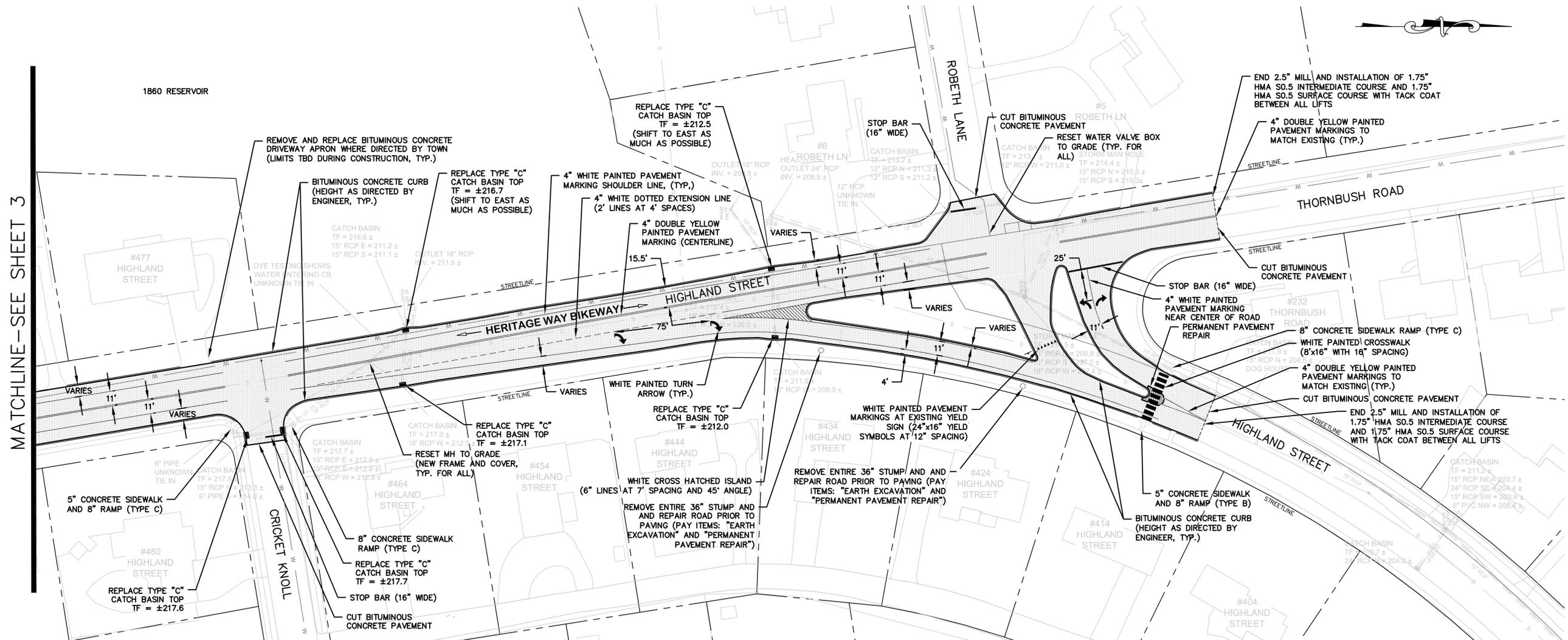
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APPROVED BY: DG		

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MATCHLINE - SEE SHEET 3



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TOWN OF WETHERSFIELD
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
 505 SILAS DEANE HIGHWAY
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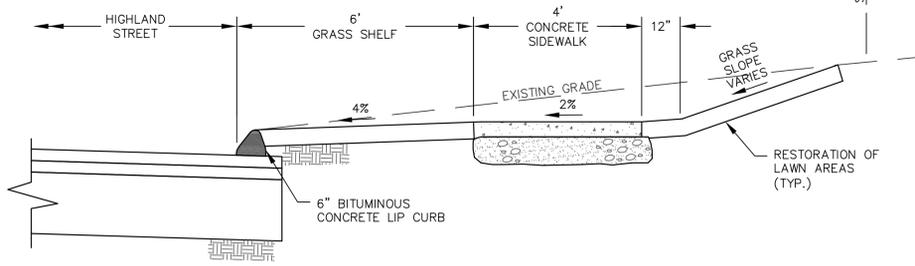
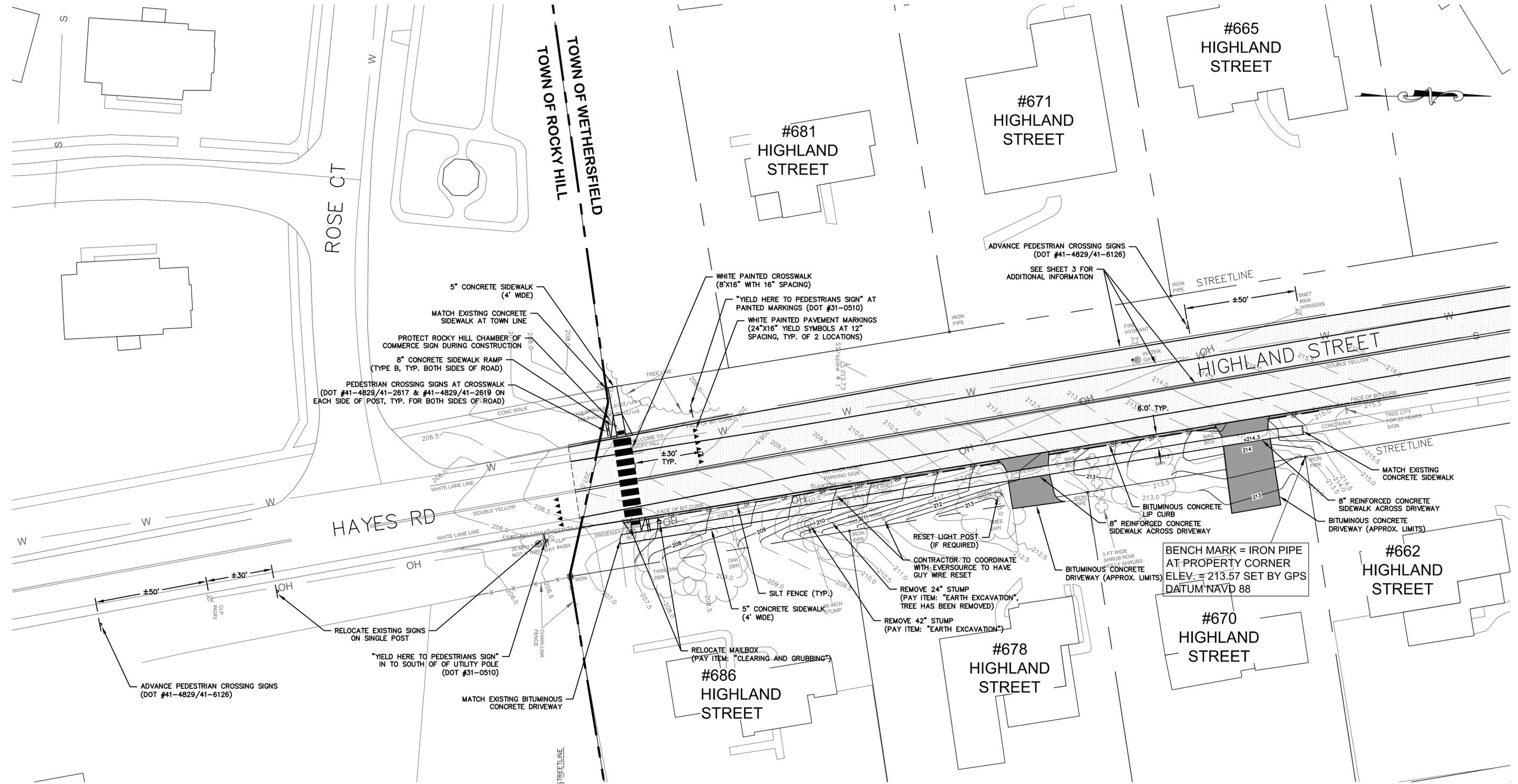
HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
 STATE PROJECT No. L159-0001

SITE PLAN

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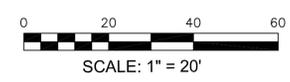
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HIGHLAND STREET SIDEWALK SECTION
(LOOKING NORTH)
NOT TO SCALE

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REVISIONS	
No.	DATE



TOWN OF WETHERSFIELD
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
505 SILAS DEANE HIGHWAY
WETHERSFIELD, CT 06109

HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
STATE PROJECT No. L159-0001

SIDEWALK EXTENSION PLAN			
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			Sheet No. 5 of 7

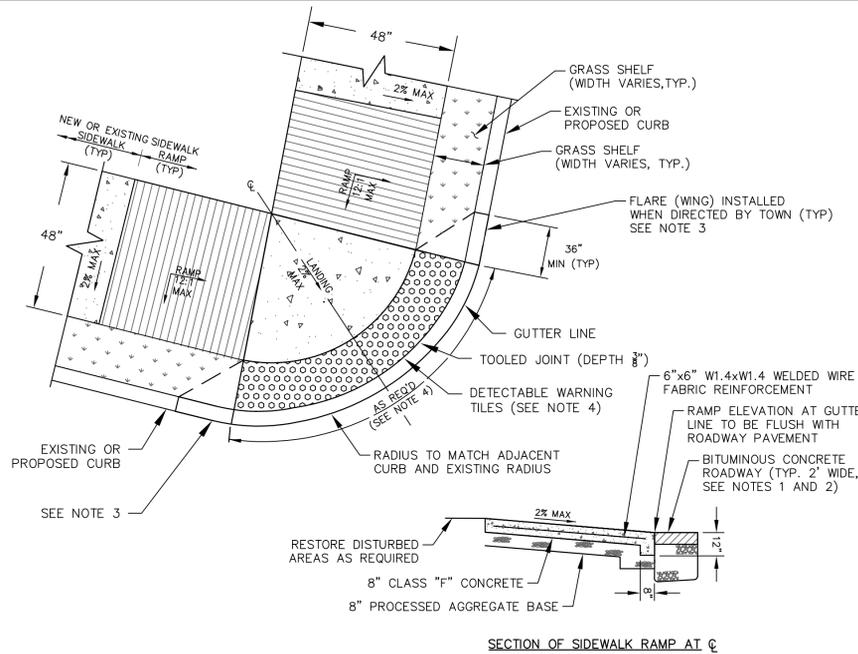
GENERAL NOTES

- LIMITED FIELD SURVEY WAS PERFORMED BY TOWN OF WETHERSFIELD DEPARTMENT OF PUBLIC WORKS, DIVISION OF ENGINEERING WITH COORDINATES BASED ON HORIZONTAL DATUM NA83 AND VERTICAL DATUM OF NAVD 1988. ADDITIONAL PLANIMETRIC INFORMATION WAS OBTAINED FROM TOWN GIS DATA AND IS CONSIDERED APPROXIMATE.
- UTILITY LOCATIONS SHOWN ARE FROM RECORD MAPS AND TOWN GIS DATA AND ARE NOT GUARANTEED TO BE ACCURATE. THE CONTRACTOR IS REQUIRED TO "CALL BEFORE YOU DIG", 1-800-922-4455, AND HAVE ALL UNDERGROUND UTILITIES PHYSICALLY MARKED ON THE GROUND IN THE AREA OF CONSTRUCTION.
- CONTRACTOR IS MADE AWARE THAT THE TOWN RESERVES THE RIGHT TO CHANGE THE SCOPE OF THE PROJECT FOR ANY REASON BEFORE OR AFTER THE BID IS AWARDED, AND MAY REDUCE ANY OR ALL QUANTITIES FOR ITEMS SHOWN ON THE BID FORM OR ELIMINATE ITEMS OF WORK ENTIRELY AS REQUIRED TO ACCOMMODATE AVAILABLE FUNDING LIMITS.
- ALL MATERIALS AND METHODS OF CONSTRUCTION SHALL BE IN ACCORDANCE WITH CONTRACT SPECIFICATIONS. FOR IMPROVEMENTS NOT SPECIFICALLY STATED ON THE CONTRACT PLANS OR IN THE SPECIFICATIONS, WORK SHALL MEET THE STATE OF CONNECTICUT, DEPARTMENT OF TRANSPORTATION, STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, FORM 818 (AS AMENDED).
- REFER TO THE CONTRACT SPECIFICATIONS FOR ADDITIONAL INFORMATION REGARDING MATERIALS AND INSTALLATION REQUIREMENTS.
- IMPLEMENTATION OF WORKER SAFETY AND HEALTH PROTOCOLS THAT ADDRESS COMPLIANCE WITH ALL RULES, LAWS AND REGULATIONS REGARDING SAFETY AND RISK OF EXPOSURE TO PHYSICAL AND CHEMICAL HAZARDS IS THE SOLE RESPONSIBILITY OF THE CONTRACTOR. ALL EMPLOYEES OF THE CONTRACTOR AND SUBCONTRACTORS ARE REQUIRED TO WEAR REFLECTIVE VESTS AND HARD HATS AT ALL TIMES WHEN ON THE PROJECT SITE.
- A PRE-CONSTRUCTION MEETING WITH TOWN STAFF IS REQUIRED PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITY.
- THE CONTRACTOR SHALL PHASE CONSTRUCTION OPERATIONS TO MINIMIZE THE SIZE OF DISTURBED AREAS AND PERIOD OF TIME THESE AREAS ARE LEFT UNSTABILIZED AND SUBJECT TO EROSION.
- THE CONTRACTOR SHALL COMMIT SUFFICIENT RESOURCES TO THE PROJECT TO ENSURE THE PROJECT IS COMPLETED WITHIN THE ALLOTTED CONTRACT TIME. ONCE MOBILIZED, THE CONTRACTOR SHALL WORK CONTINUOUSLY ON THE PROJECT UNTIL COMPLETION. ANY UNAUTHORIZED VACATING OF THE JOBSITE IS SUBJECT TO PENALTIES DESCRIBE UNDER THE "LIQUIDATED DAMAGES" SECTION OF THE CONTRACT SPECIFICATIONS.
- SEDIMENT CONTROL SYSTEMS SHALL BE INSTALLED IF DIRECTED BY THE TOWN AND MEET THE "GUIDELINES FOR SOIL EROSION AND SEDIMENT CONTROL" AS PREPARED BY THE CONNECTICUT COUNCIL ON SOIL AND WATER CONSERVATION, LATEST REVISION. THE CONTRACTOR IS RESPONSIBLE FOR THE INSTALLATION, MAINTENANCE AND REPAIR OF EROSION CONTROLS REQUIRED FOR THE PROJECT. ADDITIONAL EROSION CONTROLS SHALL BE INSTALLED BY THE CONTRACTOR FOR TEMPORARY STOCKPIILING OF EXCAVATED MATERIAL AND WHERE DEEMED NECESSARY BY THE ENGINEER. EROSION CONTROLS SHALL REMAIN IN PLACE AND MAINTAINED BY THE CONTRACTOR UNTIL THE SITE IS STABILIZED AND THE ENGINEER APPROVES THEIR REMOVAL.
- HORIZONTAL AND VERTICAL LOCATIONS OF PROPOSED WORK MAY BE ADJUSTED TO FIT EXISTING FIELD CONDITIONS WITH THE APPROVAL OF THE ENGINEER.
- THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER IF CONDITIONS ENCOUNTERED IN THE FIELD ARE DIFFERENT THAN INFORMATION SHOWN ON THE PLANS.
- THE CONTRACTOR MUST DISPOSE OF SURPLUS EXCAVATED MATERIAL OFFSITE.
- RECORD DRAWINGS WITH MARK-UPS OF ALL NEW CONSTRUCTION RELATED TO THIS PROJECT SHALL BE SUBMITTED TO THE ENGINEERING DIVISION UPON COMPLETION OF THE WORK. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROCURING ALL INFORMATION NECESSARY FOR THE TOWN TO GENERATE FINAL DRAWINGS. A REDLINED PROGRESS SET OF DRAWINGS SHALL BE MAINTAINED DAILY AND BE AVAILABLE TO THE ENGINEER AT ALL TIMES.
- THE CONTRACTOR SHALL COORDINATE WITH CT NATURAL GAS TO HAVE THEIR CREWS ADJUST GATE VALVE COVERS ON THE GAS SYSTEM AS REQUIRED.
- THE CONTRACTOR SHALL TAPER THE 3.5" OF HMA FOR AN APPROXIMATE 10' TO 15' DISTANCE TO MATCH THE EXISTING 2.5" MILLED DEPTH AT THE PAVING LIMITS. PAVEMENT THICKNESS SHALL ALSO BE TAPERED TO MATCH EXISTING SIDEWALK RAMPS TO REMAIN AND WHERE OTHERWISE DIRECTED BY THE ENGINEER.

CONCRETE SIDEWALK RAMP NOTES

- CONTRACTOR MUST CONSTRUCT SIDEWALK RAMPS TO MATCH PROPOSED PAVEMENT ELEVATIONS THAT WILL BE PROVIDED BY THE TOWN.
- ALL SIDEWALK RAMPS SHALL COMPLY WITH THE APPLICABLE STATUTES OF THE STATE OF CONNECTICUT AND FEDERAL REQUIREMENTS CONCERNING THE CONSTRUCTION OF SIDEWALK RAMPS TO MEET CURRENT AMERICANS WITH DISABILITIES (ADA) STANDARDS AND PUBLIC RIGHT-OF-WAY GUIDELINES (PROWAG).
- SIDEWALK RAMPS SHALL BE CONSTRUCTED AT ALL EXISTING AND PROPOSED PEDESTRIAN CROSSINGS AND WHERE DIRECTED BY THE TOWN. WHENEVER FEASIBLE, SEPARATE RAMPS SHALL BE PROVIDED FOR EACH PEDESTRIAN CROSSING.
- IN GENERAL, SIDEWALK RAMPS LOCATED NEAR STREET CORNERS SHALL BE CONSTRUCTED AT THE ENDS OF THE CURB RADIUS (I.E. POINTS OF CURVATURE AND TANGENT) UNLESS APPROVED OTHERWISE BY THE TOWN.
- UTILITY POLES, SIGNS AND OTHER OBSTRUCTIONS SHALL NOT BE LOCATED WITHIN THE LIMITS OF RAMPS WHENEVER FEASIBLE. IF UNAVOIDABLE, A CLEARANCE OF 36" MINIMUM MUST BE PROVIDED BETWEEN THE OBSTRUCTION AND THE BACK EDGE OF THE RAMP OR FACE OF CURB.
- SIDEWALK RAMPS SHALL BE INSTALLED ENTIRELY WITHIN THE PUBLIC RIGHT-OF-WAY. CARE SHALL BE TAKEN NOT TO DISTURB EXISTING MONUMENTATION THAT MAY PRESENT IN THE VICINITY.
- WHENEVER FEASIBLE, SIDEWALK RAMPS SHALL NOT BE INSTALLED WITHIN THE LIMITS OF PROPOSED OR EXISTING CATCH BASINS.
- THE FINAL TEXTURE OF THE CONCRETE RAMP SURFACE SHALL MATCH EXISTING, ADJACENT SIDEWALK.
- CARE SHALL BE TAKEN TO ASSURE UNIFORM GRADE ON THE RAMP, FREE OF SAGS, LIPS AND ABRUPT GRADE CHANGES.
- THE HAUNCH AT THE BOTTOM OF THE RAMP SHALL BE POURED INTEGRALLY WITH THE RAMP, SET FLUSH WITH THE PROPOSED GUTTER LINE, AND BEVELED.
- WHEREVER FEASIBLE, MAXIMUM SLOPES OF ADJOINING GUTTERS AND ROAD SURFACES IMMEDIATELY ADJACENT TO THE SIDEWALK RAMP OR ACCESSIBLE ROUTE SHALL NOT EXCEED 5% MAXIMUM COMBINED GRADE OF RAMP AND ADJOINING SURFACE SHALL NOT EXCEED 13%.
- REMOVAL AND REPLACEMENT OF EXISTING SIDEWALK TO ACCOMMODATE NEW RAMP INSTALLATIONS SHALL BE TO THE NEAREST EXPANSION/CONTRACTION JOINT OR DUMMY JOINT THAT PROVIDES A MAXIMUM 12:1 SLOPE AS SHOWN IN THE DETAIL OR 15', WHICHEVER IS CLOSER, UNLESS OTHERWISE DIRECTED BY THE TOWN.
- SIDEWALK RAMPS AT MARKED CROSSINGS SHALL BE

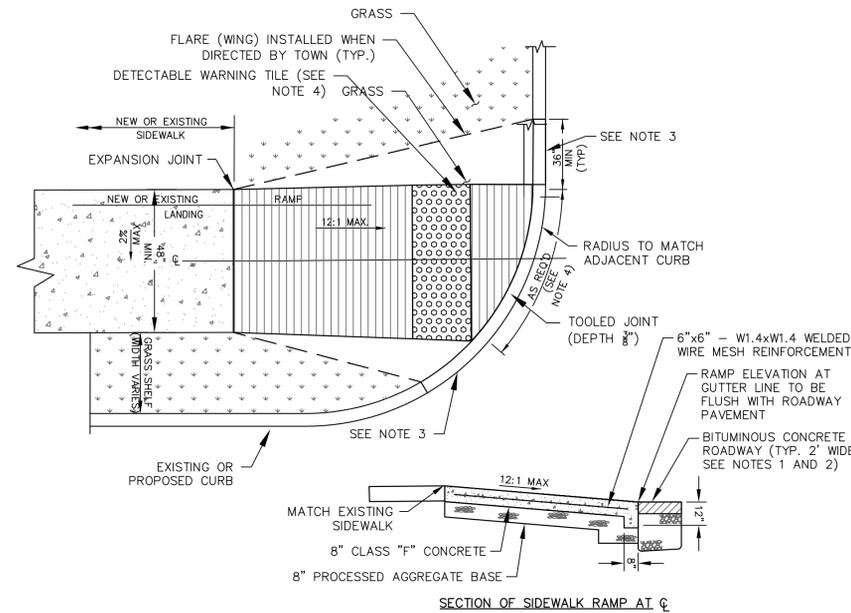
- COMPLETELY CONTAINED WITHIN THE MARKINGS, EXCLUDING ANY FLARED SIDES.
- EXPANSION JOINTS IN CONCRETE SHALL MATCH THOSE IN ADJACENT SIDEWALKS BUT IN NO CASE SHALL THE SPACING BETWEEN EXPANSION JOINTS EXCEED 15' UNLESS OTHERWISE DIRECTED BY THE TOWN.
- RAISED ISLANDS IN CROSSINGS SHALL HAVE SIDEWALK RAMPS AT BOTH SIDES AND A LEVEL AREA WITH A 4 FT MINIMUM LENGTH BETWEEN THE RAMPS IN THE AREA OF THE ISLAND INTERSECTED BY THE CROSSINGS. IF THIS CANNOT BE ACHIEVED, THE RAISED ISLAND SHALL BE CUT THROUGH LEVEL WITH THE ROADWAY.
- DETECTABLE WARNING TILES SHALL BE 24" X 48" MIN. VPC, CAST-IN-PLACE, REPLACEABLE PANEL WITH 8-BOLT PATTERN AS MANUFACTURED BY ADA SOLUTIONS, INC., P.O. BOX 3, NORTH BILLERICA, MA 01862 (1-800-372-0519) OR APPROVED EQUAL. TILES MUST BE CUT AS REQUIRED PER APPLICABLE DETAILS AND BRICK RED COLOR CONFORMING TO FEDERAL STANDARDS, WHICH IS HOMOGENEOUS THROUGHOUT THE TILE.
- INSTALL EDGE OF DETECTABLE WARNING TILES 6" FROM THE EDGE OF ROAD AND ALIGN DOMES ON A SQUARE GRID IN THE DIRECTION OF PEDESTRIAN TRAVEL.
- ANY QUESTIONS PERTAINING TO LOCATION AND DIMENSIONS OF CONCRETE SIDEWALK RAMPS AND/OR PLACEMENT OF DETECTABLE WARNING TILES SHALL BE DIRECTED TO THE TOWN PRIOR TO INSTALLATION.
- LIMITS OF DISTURBANCE TO ADJACENT DRIVEWAY AND LAWN AREAS SHALL BE MINIMIZED DURING CONSTRUCTION.
- IMMEDIATELY NOTIFY THE TOWN IF EXISTING TREE ROOTS ARE ENCOUNTERED DURING THE WORK TO DISCUSS PROPER REMOVAL TO AVOID DAMAGING THE ASSOCIATED TREE.
- ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.
- SEE ASSOCIATED SIDEWALK RAMP DETAILS FOR ADDITIONAL INFORMATION.



- NOTES:**
- NEW PAVEMENT SHALL MATCH EXISTING PAVEMENT THICKNESS OR BE INSTALLED WITH A 5" MIN. THICKNESS.
 - PROCESSED AGGREGATE BASE SHALL BE INSTALLED TO PROVIDE 12" MIN. THICKNESS BELOW THE BOTTOM OF THE EXISTING PAVEMENT SECTION.
 - TRANSITION BITUMINOUS CONCRETE CURB TO MATCH SIDEWALK RAMP. IF CONCRETE FLARES (WINGS) ARE NECESSARY TO ACCOMMODATE ADJACENT GRADES, FLARES AND CONCRETE CURB SHALL BE POURED AT 10% MAX SLOPE AND PAID WITH SIDEWALK RAMP.
 - POUR CONCRETE RAMP TO PROVIDE A 6" CLEARANCE BETWEEN DETECTABLE WARNING TILE AND EDGE OF ROAD.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.
 - REFER TO CONCRETE SIDEWALK RAMP NOTES FOR ADDITIONAL INFORMATION.

CONCRETE SIDEWALK RAMP TYPE A

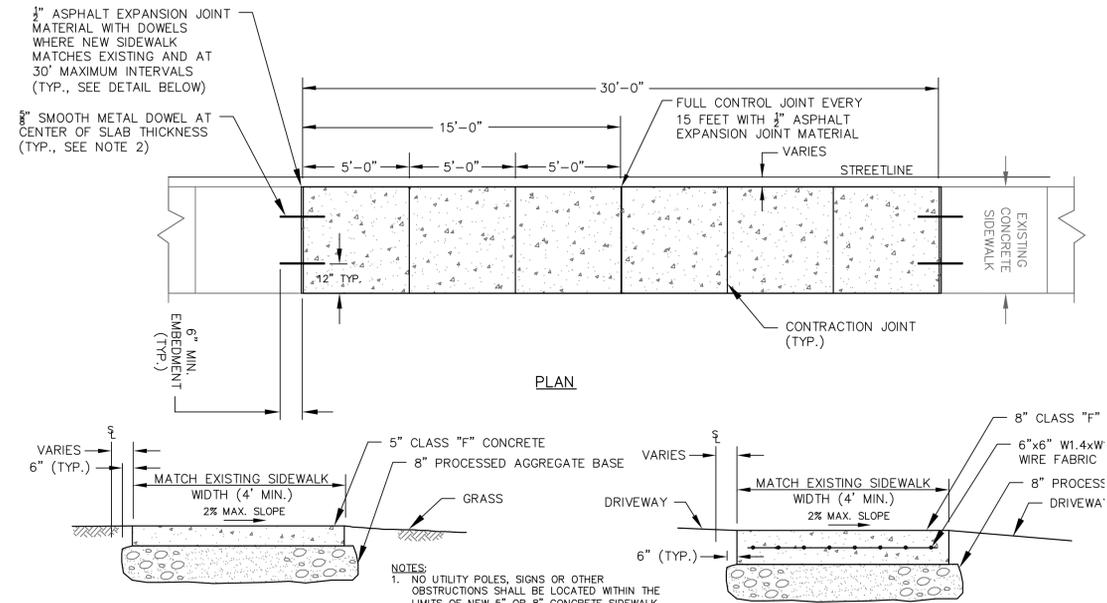
NOT TO SCALE



- NOTES:**
- NEW PAVEMENT SHALL MATCH EXISTING PAVEMENT THICKNESS OR BE INSTALLED WITH A 4" MIN. THICKNESS, EXCEPT FOR MAJOR TOWN ROADS LISTED ON SHEET 14 WHERE A 5" MIN. PAVEMENT THICKNESS IS REQUIRED.
 - PROCESSED AGGREGATE BASE SHALL BE INSTALLED TO PROVIDE 12" MIN. THICKNESS BELOW THE BOTTOM OF THE EXISTING PAVEMENT SECTION.
 - TRANSITION BITUMINOUS CONCRETE CURB TO MATCH SIDEWALK RAMP. IF CONCRETE FLARES (WINGS) ARE NECESSARY TO ACCOMMODATE ADJACENT GRADES, FLARES AND CONCRETE CURB SHALL BE POURED AT 10% MAX. SLOPE AND PAID WITH SIDEWALK RAMP.
 - POUR CONCRETE RAMP TO PROVIDE A 6" CLEARANCE BETWEEN DETECTABLE WARNING TILE AND EDGE OF ROAD.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO CTDOT FORM 817 STANDARDS.
 - REFER TO CONCRETE SIDEWALK RAMP NOTES FOR ADDITIONAL INFORMATION.

CONCRETE SIDEWALK RAMP - TYPE B

NOT TO SCALE

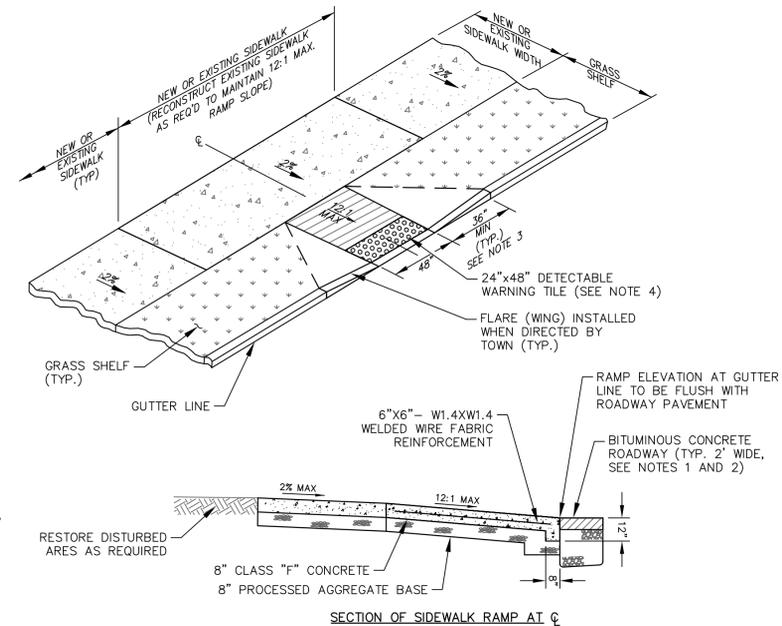


SECTION VIEW OF 5\"/>

- NOTES:**
- NO UTILITY POLES, SIGNS OR OTHER OBSTRUCTIONS SHALL BE LOCATED WITHIN THE LIMITS OF NEW 5" OR 8" CONCRETE SIDEWALK. DOWELS SHALL BE DIPPED IN LIQUID ASPHALT AND AN APPROVED BOND BREAKER OR PLASTIC SLEEVE MUST BE PROVIDED ON ONE SIDE OF THE JOINT PRIOR TO INSTALLATION.
 - LIMITS OF DISTURBANCE TO ADJACENT LAWN AREAS AND DRIVEWAYS SHALL BE MINIMIZED DURING CONSTRUCTION.
 - REINFORCED CONCRETE SIDEWALK MUST BE INSTALLED ACROSS ALL DRIVEWAYS TO ACCOMMODATE VEHICLE LOADS. SEE BITUMINOUS CONCRETE DRIVEWAY APRON DETAIL FOR ASSOCIATED REQUIREMENTS.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO CTDOT FORM 818 STANDARDS.

CONCRETE SIDEWALK

NOT TO SCALE



SECTION OF SIDEWALK RAMP AT C

- NOTES:**
- NEW PAVEMENT SHALL MATCH EXISTING PAVEMENT THICKNESS OR BE INSTALLED WITH A 4" MIN. THICKNESS, EXCEPT FOR MAJOR TOWN ROADS LISTED ON SHEET 14 WHERE A 5" MIN. PAVEMENT THICKNESS IS REQUIRED.
 - PROCESSED AGGREGATE BASE SHALL BE INSTALLED TO PROVIDE 12" MIN. THICKNESS BELOW THE BOTTOM OF THE EXISTING PAVEMENT SECTION.
 - TRANSITION BITUMINOUS CONCRETE CURB TO MATCH SIDEWALK RAMP. IF CONCRETE FLARES (WINGS) ARE NECESSARY TO ACCOMMODATE ADJACENT GRADES, FLARES AND CONCRETE CURB SHALL BE POURED AT 10% MAX. SLOPE AND PAID WITH SIDEWALK RAMP.
 - POUR CONCRETE RAMP TO PROVIDE A 6" CLEARANCE BETWEEN DETECTABLE WARNING TILE AND EDGE OF ROAD.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO CTDOT FORM 817 STANDARDS.
 - REFER TO CONCRETE SIDEWALK RAMP NOTES FOR ADDITIONAL INFORMATION.

CONCRETE SIDEWALK RAMP - TYPE C

NOT TO SCALE

REVISIONS		
No.	DATE	COMMENTS

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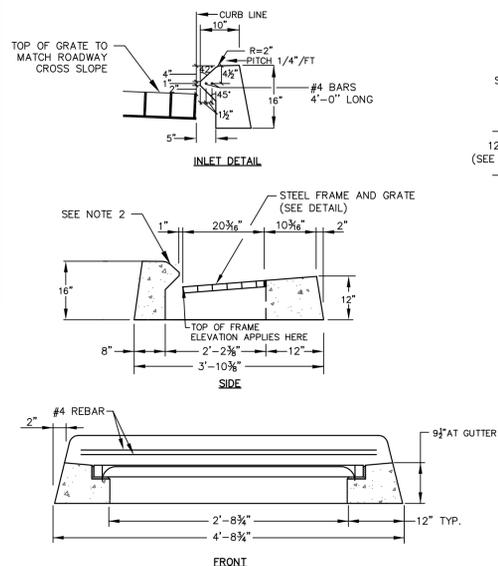
TOWN OF WETHERSFIELD
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
 505 SILAS DEANE HIGHWAY
 WETHERSFIELD, CT 06109

HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
 STATE PROJECT No. L159-0001

CONSTRUCTION DETAILS

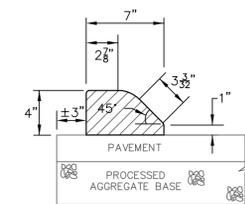
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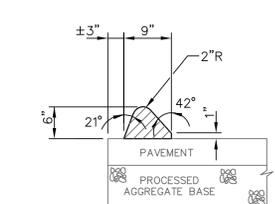


- NOTES:**
- CONCRETE TOP SHALL BE CAST TO MATCH ADJACENT BITUMINOUS CONCRETE LIP CURB.
 - TOP OF ALL CONCRETE CURB INLETS SHALL BE CAST WITH THE WORDS "DRAINS TO WATERWAY" WITH 2" LETTERS THAT ARE 3" DEEP.
 - CATCH BASIN TOP DIMENSIONS PROVIDED ARE APPROXIMATE. ACTUAL DIMENSIONS ARE SUBJECT TO APPROVAL BY THE TOWN.

TYPE "C" CATCH BASIN TOP
NOT TO SCALE

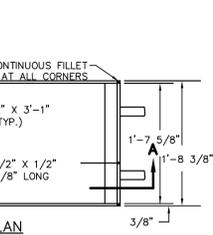
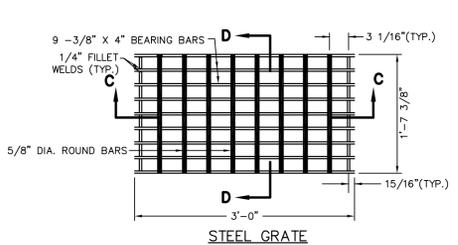


4" BITUMINOUS CONCRETE PARK CURB
NOT TO SCALE



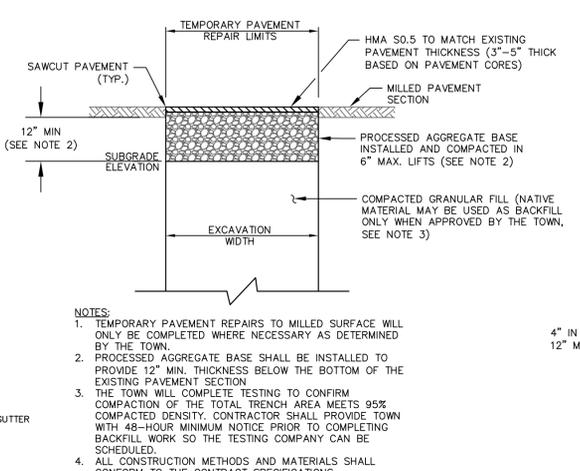
6" BITUMINOUS CONCRETE LIP CURB
NOT TO SCALE

- CURB NOTES:**
- ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.
 - BITUMINOUS CONCRETE CURB SHALL BE INSTALLED ON THE BINDER COURSE OF PAVEMENT WHENEVER POSSIBLE.



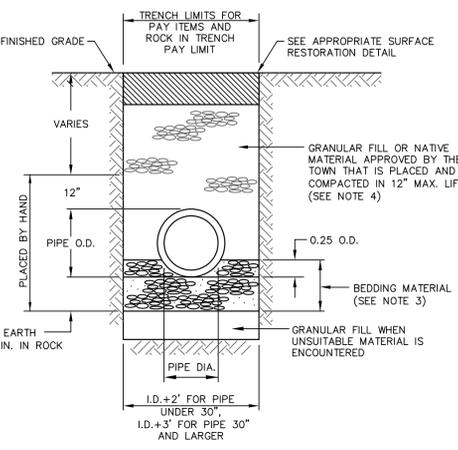
- NOTES:**
- ALL BARS SHALL BE WELDED AT ALL INTERSECTIONS.
 - FRAMES AND GRATES SHALL BE GALVANIZED IN ACCORDANCE WITH THE STATE OF CONNECTICUT SPECIFICATIONS FORM B17 SECTION M.06.03.

STEEL CATCH BASIN FRAME AND GRATE
NOT TO SCALE



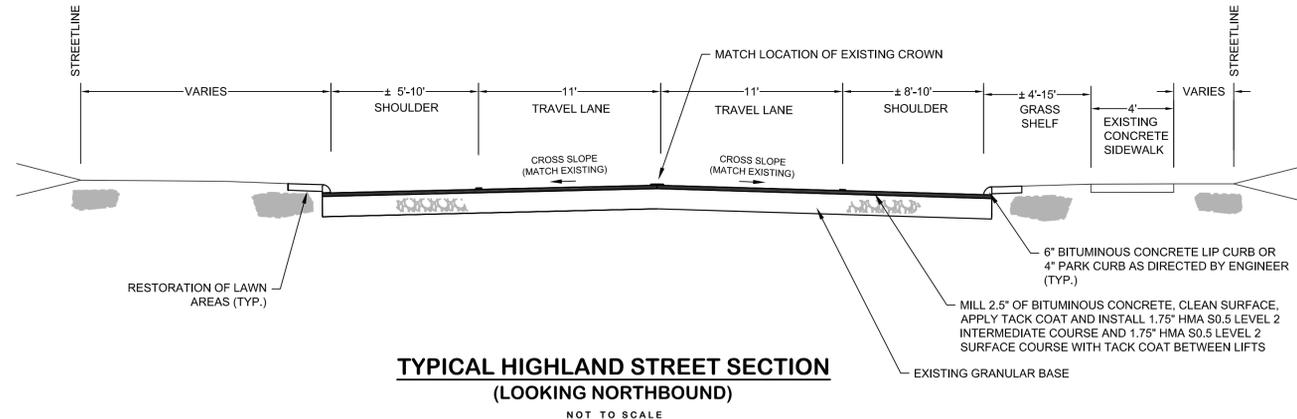
- NOTES:**
- TEMPORARY PAVEMENT REPAIRS TO MILLED SURFACE WILL ONLY BE COMPLETED WHERE NECESSARY AS DETERMINED BY THE TOWN.
 - PROCESSED AGGREGATE BASE SHALL BE INSTALLED TO PROVIDE 12" MIN. THICKNESS BELOW THE BOTTOM OF THE EXISTING PAVEMENT SECTION.
 - THE TOWN WILL COMPLETE TESTING TO CONFIRM COMPACTION OF THE TOTAL TRENCH AREA MEETS 95% COMPACTED DENSITY. CONTRACTOR SHALL PROVIDE TOWN WITH 48-HOUR MINIMUM NOTICE PRIOR TO COMPLETING BACKFILL WORK SO THE TESTING COMPANY CAN BE SCHEDULED.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.

TEMPORARY PAVEMENT REPAIR (MILLED SURFACE)
NOT TO SCALE

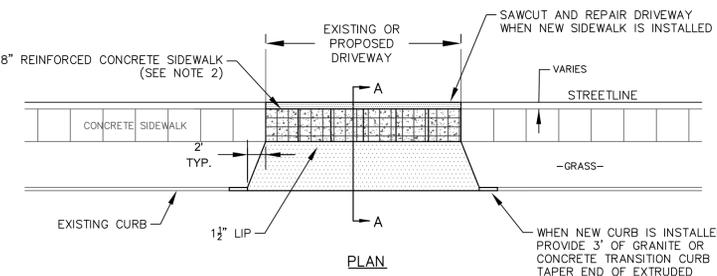


- NOTES:**
- ALL CONCRETE PIPE TO BE MINIMUM CLASS IV WITH 2 FT OF COVER UNLESS OTHERWISE APPROVED BY THE ENGINEER.
 - USE WATERTIGHT RUBBER GASKETS IN ALL PIPE JOINTS.
 - BEDDING MATERIAL SHALL BE INSTALLED A MINIMUM OF 12" ABOVE THE TOP OF ALL PLASTIC PIPES AND PIPES 48" IN DIAMETER AND LARGER.
 - THE TOWN WILL COMPLETE TESTING TO CONFIRM COMPACTION OF THE TOTAL TRENCH AREA MEETS 95% COMPACTED DENSITY. CONTRACTOR SHALL PROVIDE TOWN WITH 48-HOUR MINIMUM NOTICE PRIOR TO COMPLETING BACKFILL WORK SO THE TESTING COMPANY CAN BE SCHEDULED.
 - EXCAVATION, BEDDING MATERIAL AND BACKFILL ARE INCLUDED IN CONTRACT UNIT PRICE BID FOR "CULVERT" OF THE TYPE SPECIFIED.

STORM SEWER TRENCH
NOT TO SCALE

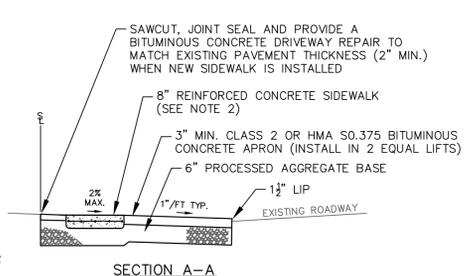


TYPICAL HIGHLAND STREET SECTION (LOOKING NORTHBOUND)
NOT TO SCALE



- NOTES:**
- SEE RESPECTIVE DETAILS FOR CONCRETE SIDEWALK AND CURB REQUIREMENTS.
 - REINFORCED CONCRETE SIDEWALK MUST BE INSTALLED ACROSS DRIVEWAY LIMITS TO ACCOMMODATE VEHICLE LOADS. REFER TO ASSOCIATED DETAIL FOR ADDITIONAL INFORMATION.
 - MINIMUM DISTANCE BETWEEN MULTIPLE DRIVEWAY OPENINGS SHALL BE 10'.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.

BITUMINOUS CONCRETE DRIVEWAY APRON
NOT TO SCALE

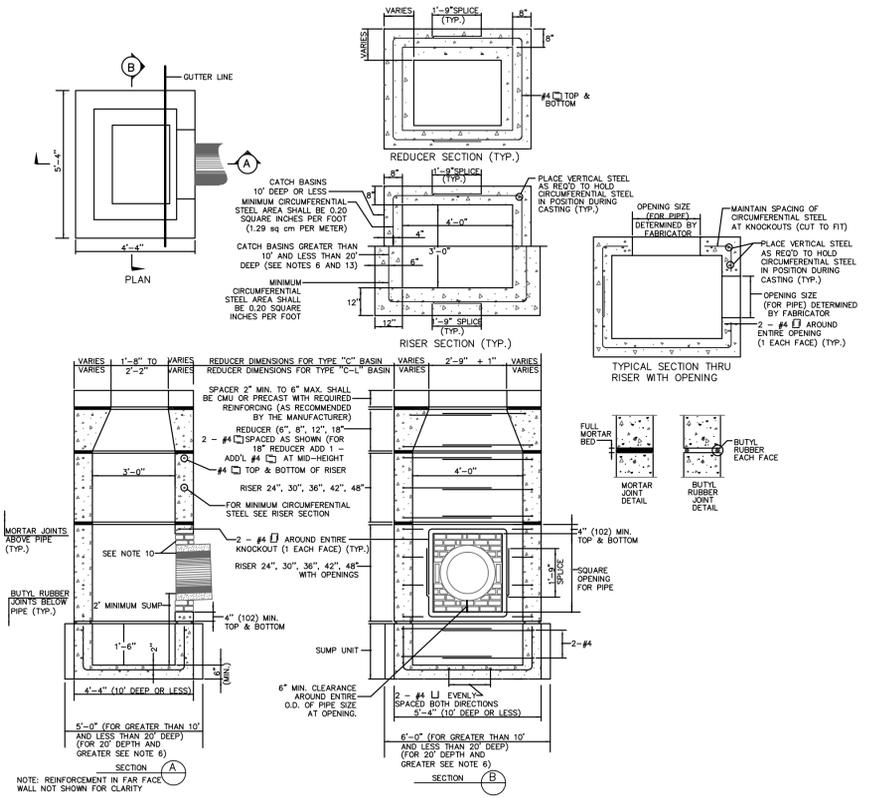


- NOTES:**
- NEW PAVEMENT SHALL MATCH EXISTING PAVEMENT THICKNESS OR BE INSTALLED WITH A 5" MIN. THICKNESS.
 - ALL CONSTRUCTION METHODS AND MATERIALS SHALL CONFORM TO THE CONTRACT SPECIFICATIONS.
 - THIS DETAIL APPLIES TO PAVEMENT REPAIR WORK COMPLETED PRIOR TO MILLING (I.E. AROUND CB FRAMES, ALONG SIDEWALK RAMP, ETC.).

PERMANENT PAVEMENT REPAIR
NOT TO SCALE

- CATCH BASIN NOTES:**
- REINFORCEMENT SHALL CONFORM TO ASTM A615, GRADE 60. CATCH BASIN DETAILS SHOW STANDARD REINFORCEMENT. WELDED WIRE FABRIC WITH AN AREA EQUAL TO OR GREATER THAN THE REINFORCING SHOWN MAY BE SUBSTITUTED.
 - ALL LAP SPICES, DEVELOPMENT LENGTHS, BENDS FOR REINFORCEMENT, AND WELDED WIRE FABRIC SHALL CONFORM TO AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES.
 - ALL REINFORCEMENT SHALL HAVE A MINIMUM CLEAR COVER OF 2".
 - MINIMUM CONCRETE COMPRESSIVE STRENGTH $F_c' = 4000$ PSI SHALL BE OBTAINED PRIOR TO SHIPPING.
 - BASES AND RISERS AT A DEPTH OF 20' OR MORE SHALL BE DESIGNED BY THE CONTRACTOR AND WORKING DRAWINGS SHALL BE SUBMITTED TO THE TOWN FOR REVIEW AND APPROVAL. SEE APPROPRIATE DETAIL FOR CATCH BASIN FRAMES AND GRATES.
 - RISERS MAY BE PREFABRICATED WITH PIPE OPENINGS IN ALL FOUR WALLS. ADEQUATE REINFORCING AROUND PIPE OPENINGS CONFORMING TO THESE PLANS SHALL BE PROVIDED. ANY RISERS USED WHERE A PIPE OPENING IS TO REMAIN IN PLACE, MUST BE FORMED UP WITH BRICK AS DIRECTED BY THE TOWN.
 - RISERS SHALL NEVER HAVE CORNER PIPE ENTRIES.
 - ALL OPENINGS SURROUNDING PIPES SHALL BE CLOSED USING CEMENT RUBBLE MASONRY CONFORMING TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M.11.02. IF THE TOWN DETERMINES THAT THE CLOSURE OF ANY PIPE OPENING IS UNSATISFACTORY, THE CONTRACTOR SHALL IMMEDIATELY RECLOSE SAID OPENING. THE LOCATION OF PIPE OPENINGS SHALL NOT REDUCE THE WALL THICKNESS.
 - THE LATEST CONNECTICUT D.O.T. STANDARD SPECIFICATIONS AND SUPPLEMENTALS SHALL GOVERN.
 - WALL THICKNESS OF ALL CATCH BASINS OVER 10' DEEP SHALL BE INCREASED TO 12" THICK. INSIDE DIMENSION SHALL REMAIN THE SAME (THE 12" THICKNESS WILL START AFTER THE FIRST 10').
 - BUTYL RUBBER JOINT SEAL SHALL CONFORM TO AASHTO M-198 AND MORTAR SHALL CONFORM TO THE LATEST STATE OF CONNECTICUT STANDARD SPECIFICATIONS MATERIAL SECTION M.11.04. ALL CATCH BASINS SHALL BE BUILT WITH SPACERS AS SHOWN IN THE DETAILS.
 - SHRINKAGE AND TEMPERATURE REINFORCEMENT SHALL BE PROVIDED IN THE TOPS OF SLABS. THE TOTAL AREA OF REINFORCEMENT PROVIDED SHALL BE AT LEAST 0.125 SQUARE INCHES PER FOOT IN EACH DIRECTION. THE MAXIMUM SPACING OF THIS REINFORCEMENT SHALL NOT EXCEED 18 INCHES.

CATCH BASIN NOTES



TYPE "C" AND TYPE "C-L" CATCH BASIN
NOT TO SCALE



TOWN OF WETHERSFIELD
DEPARTMENT OF PUBLIC WORKS
ENGINEERING DIVISION
505 SILAS DEANE HIGHWAY
WETHERSFIELD, CT 06109

HIGHLAND STREET PAVEMENT REHABILITATION
ROCKY HILL TOWN LINE TO THORNBUSH ROAD
STATE PROJECT No. L159-0001

CONSTRUCTION DETAILS

REVISIONS		
No.	DATE	COMMENTS

DWG FILE: HIGHLANDSTREHAB-SFDPLAN.DWG
DRAWN BY: LB
CHECKED BY: DG
APPROVED BY: DG
DATE: MAY 2020
SCALE: 1" = 20' OR AS NOTED
Sheet No. **7 of 7**

90% DESIGN SUBMITTAL

C:\Projects\Highland Street Road Rehab - Town Line to Thornbush\Design\Final Design\90% Submittal\HighlandStreetRehab-SFDPlan.dwg, 5/8/2020 8:17:06 AM, dgeger